

List of pages in this Trip Kit

Trip Kit Index

Airport Information For VVPQ

Terminal Charts For VVPQ

Revision Letter For Cycle 02-2019

Change Notices

Notebook

General Information

Location: PHU QUOC VNM
ICAO/IATA: VVPQ / PQC
Lat/Long: N10° 10.18', E103° 59.58'
Elevation: 23 ft

Airport Use: Public
Daylight Savings: Not Observed
UTC Conversion: -7:00 = UTC
Magnetic Variation: 0.2° W

Fuel Types: Jet A-1
Customs: Yes
Airport Type: IFR
Landing Fee: Yes
Control Tower: Yes
Jet Start Unit: No
LLWS Alert: No
Beacon: Yes

Sunrise: 2327 Z
Sunset: 1109 Z

Runway Information

Runway: 10
Length x Width: 9843 ft x 148 ft
Surface Type: bitu
TDZ-Elev: 13 ft
Lighting: Edge, ALS, Centerline

Runway: 28
Length x Width: 9843 ft x 148 ft
Surface Type: bitu
TDZ-Elev: 23 ft
Lighting: Edge, ALS, Centerline

Communication Information

Phu Quoc Intl Tower: 118.600
Ho Chi Minh Control: 120.900 RCO

VVPQ/PQC
PHU QUOC INTL



PHU QUOC, VIETNAM

24 APR 15

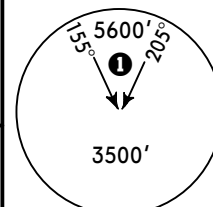
10-2

Eff 30 Apr

STAR

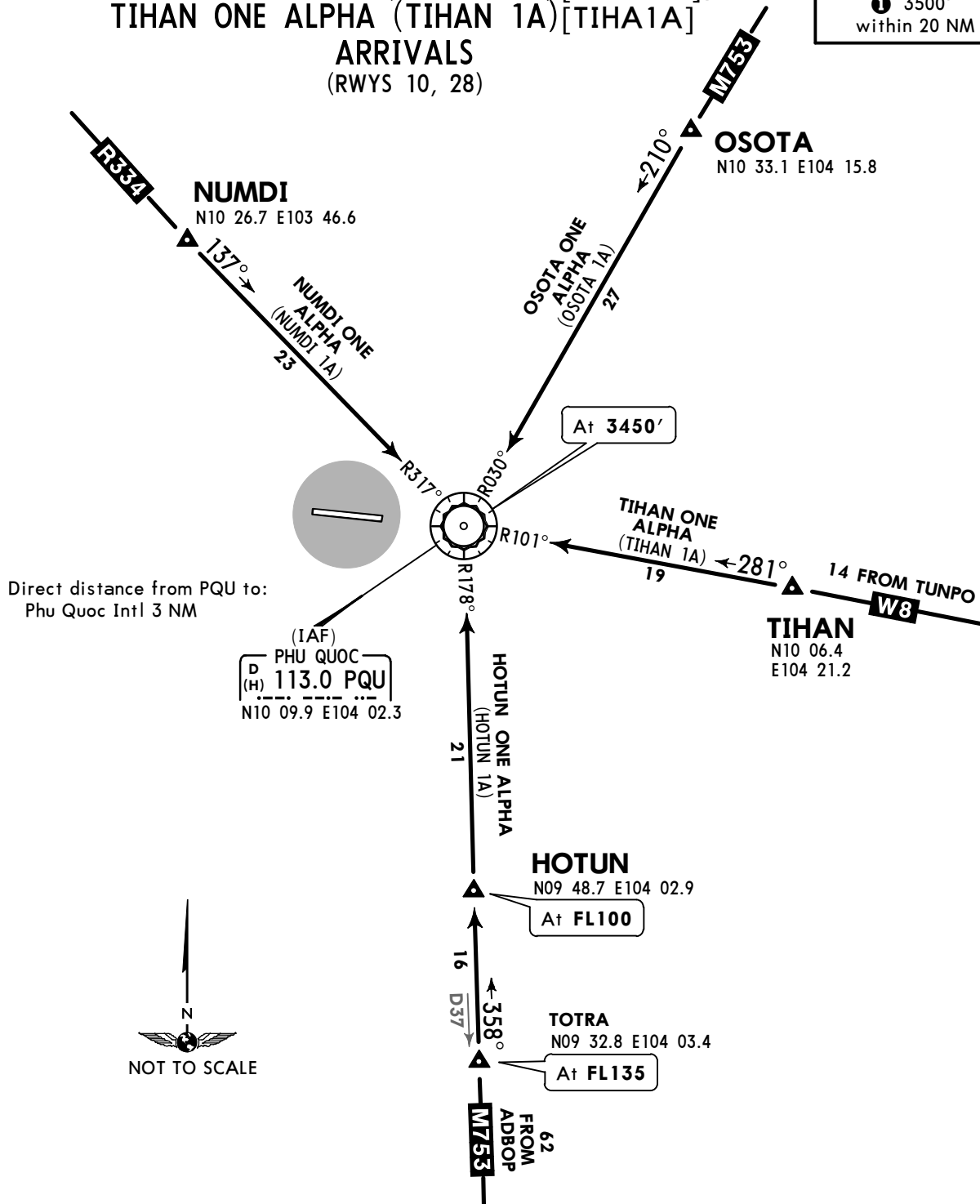
Apt Elev
23'

Alt Set: hPa Trans level: FL100 Trans alt: 9030'
In case of deviation more than 5 NM from inbound track, aircraft shall inform Phu Qhoc TWR, MAINTAIN MSA and proceed to PQU for approach.



MSA PQU VOR
① 3500'
within 20 NM

HOTUN ONE ALPHA (HOTUN 1A) [HOTU1A],
NUMDI ONE ALPHA (NUMDI 1A) [NUMD1A],
OSOTA ONE ALPHA (OSOTA 1A) [OSOT1A],
TIHAN ONE ALPHA (TIHAN 1A) [TIHA1A]
ARRIVALS
(RWYS 10, 28)



STAR

ROUTING

HOTUN ONE ALPHA	Descend from FL135 at TOTRA to FL100 at HOTUN. Proceed on PQU R-178, descend to 3450' at PQU for approach procedures VOR RWY 10/28 or ILS RWY 10/28.
NUMDI ONE ALPHA	From NUMDI descend to 3450' at PQU for approach procedures VOR RWY 10/28 or ILS RWY 10/28.
OSOTA ONE ALPHA	From OSOTA proceed on PQU R-030, descend to 3450' at PQU for approach procedures VOR RWY 10/28 or ILS RWY 10/28.
TIHAN ONE ALPHA	From TIHAN proceed on PQU R-101, descend to 3450' at PQU for approach procedures VOR RWY 10/28 or ILS RWY 10/28.

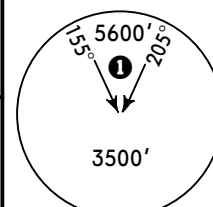
VVPQ/PQC
PHU QUOC INTL

JEPPESSEN 24 APR 15 10-3 Eff 30 Apr

PHU QUOC, VIETNAM

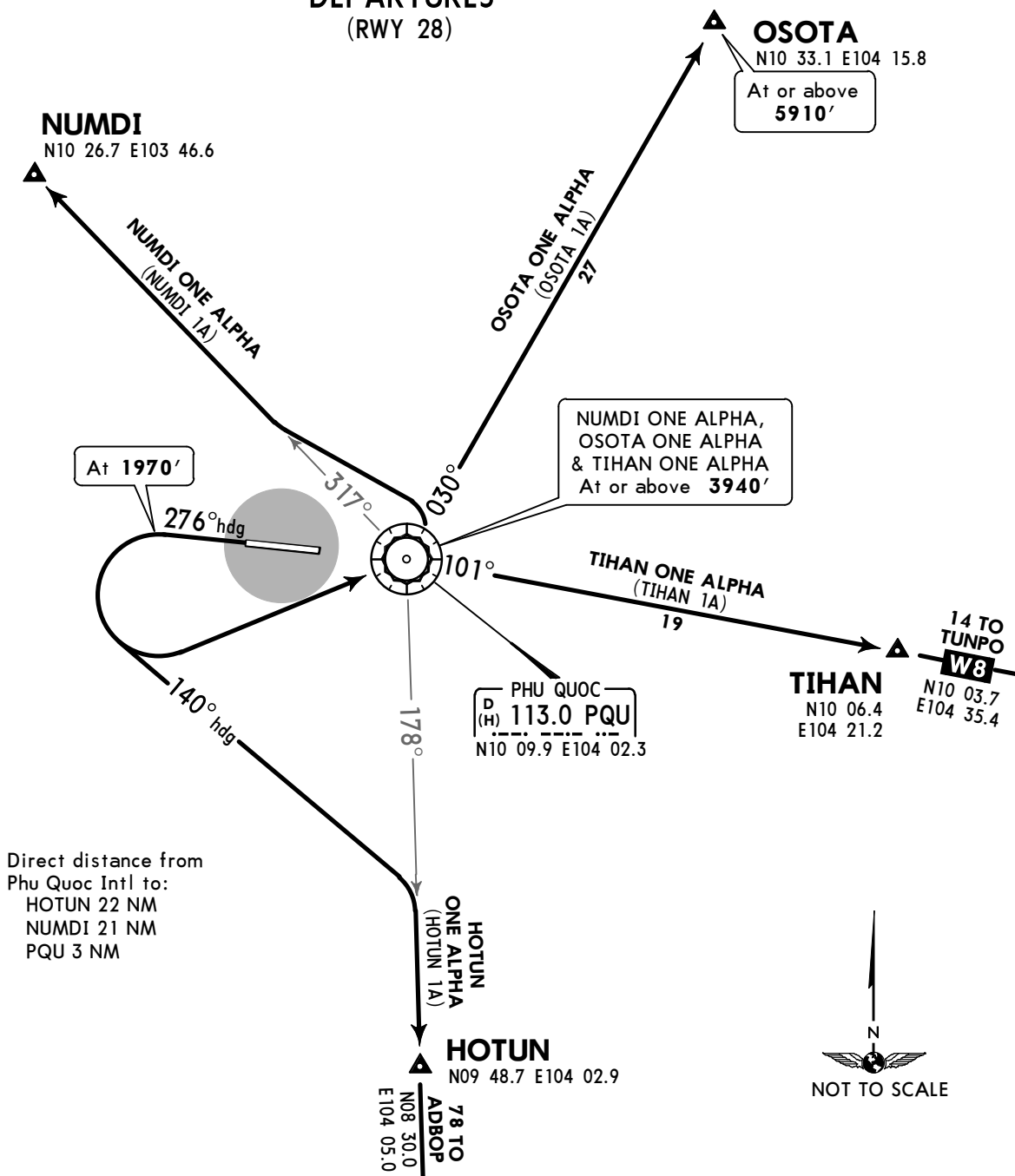
SID

Apt Elev 23' Trans level: FL100 Trans alt: 9030'



MSA PQU VOR
① 3500'
within 20 NM

HOTUN ONE ALPHA (HOTUN 1A) [HOTU1A],
NUMDI ONE ALPHA (NUMDI 1A) [NUMD1A],
OSOTA ONE ALPHA (OSOTA 1A) [OSOT1A],
TIHAN ONE ALPHA (TIHAN 1A) [TIHA1A]
DEPARTURES
(RWY 28)



INITIAL CLIMB

After take-off, MAINTAIN runway heading until reaching 1970'.

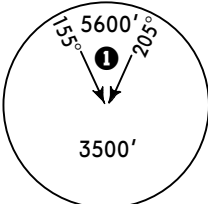
SID	ROUTING
HOTUN ONE ALPHA	At 1970' turn LEFT on heading 140°, intercept PQU R-178 to HOTUN.
NUMDI ONE ALPHA	At 1970' turn LEFT to PQU, then turn LEFT to intercept PQU R-317 to NUMDI.
OSOTA ONE ALPHA	At 1970' turn LEFT to PQU, then intercept PQU R-030 to OSOTA.
TIHAN ONE ALPHA	At 1970' turn LEFT to PQU, then proceed on PQU R-101 and intercept W-8 at TIHAN.

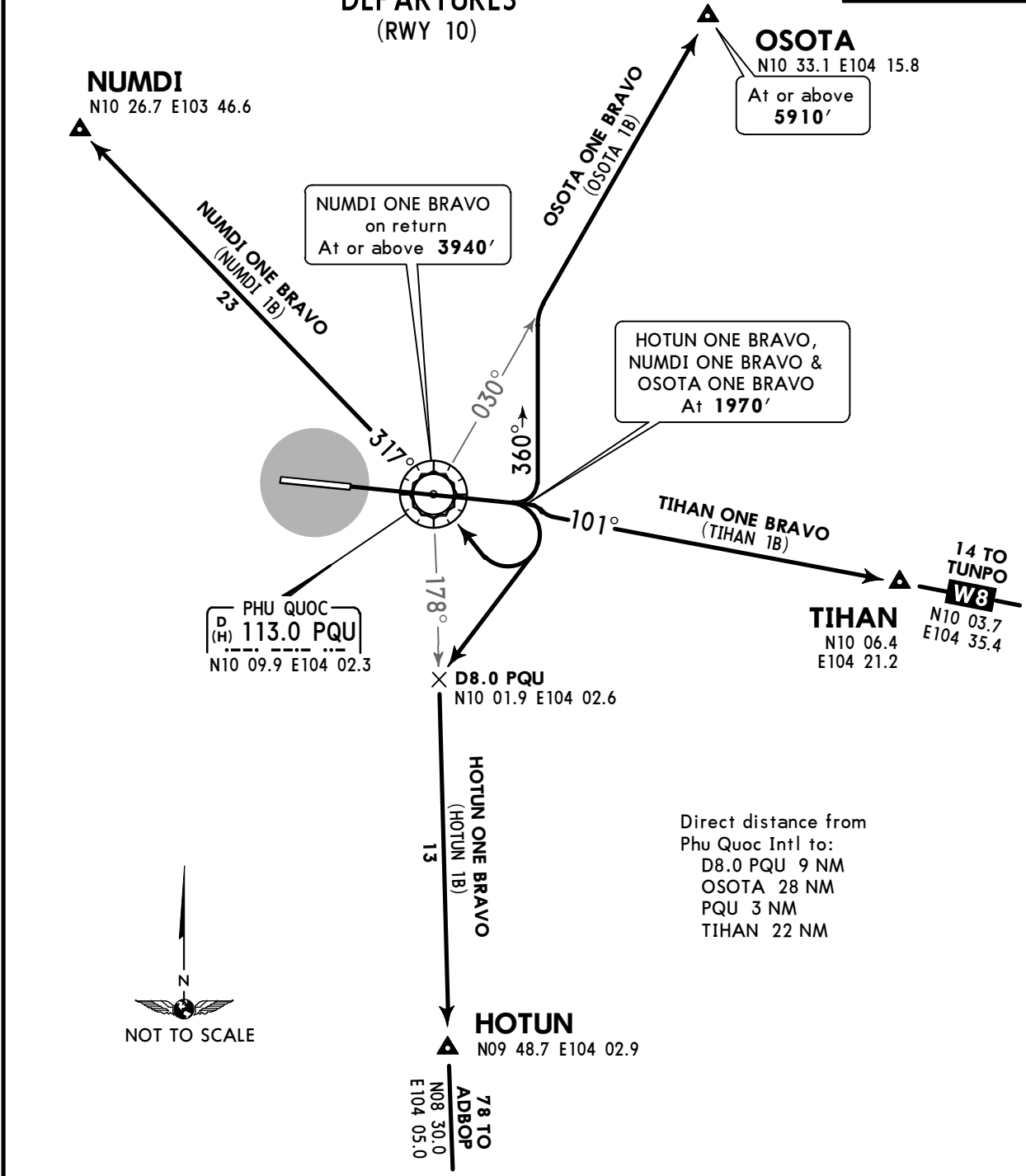
VVPQ/PQC
PHU QUOC INTL

JEPESEN 24 APR 15 10-3A Eff 30 Apr

PHU QUOC, VIETNAM

SID

Apt Elev 23'	Trans level: FL100 Trans alt: 9030'	
<p>HOTUN ONE BRAVO (HOTUN 1B)[HOTU1B], NUMDI ONE BRAVO (NUMDI 1B)[NUMD1B], OSOTA ONE BRAVO (OSOTA 1B)[OSOT1B], TIHAN ONE BRAVO (TIHAN 1B)[TIHA1B]</p> <p>DEPARTURES (RWY 10)</p>		<p>MSA PQU VOR ① 3500' within 20 NM</p>



INITIAL CLIMB

After take-off, MAINTAIN runway heading.

SID	ROUTING
HOTUN ONE BRAVO	When reaching 1970', turn RIGHT to intercept PQU R-178 at D8.0 PQU to HOTUN.
NUMDI ONE BRAVO	When reaching 1970', turn RIGHT to PQU, intercept PQU R-317 to NUMDI.
OSOTA ONE BRAVO	When reaching 1970', turn LEFT on track 360° to intercept PQU R-030 to OSOTA.
TIHAN ONE BRAVO	After passing PQU, intercept PQU R-101 to TIHAN.

VVPQ/PQC

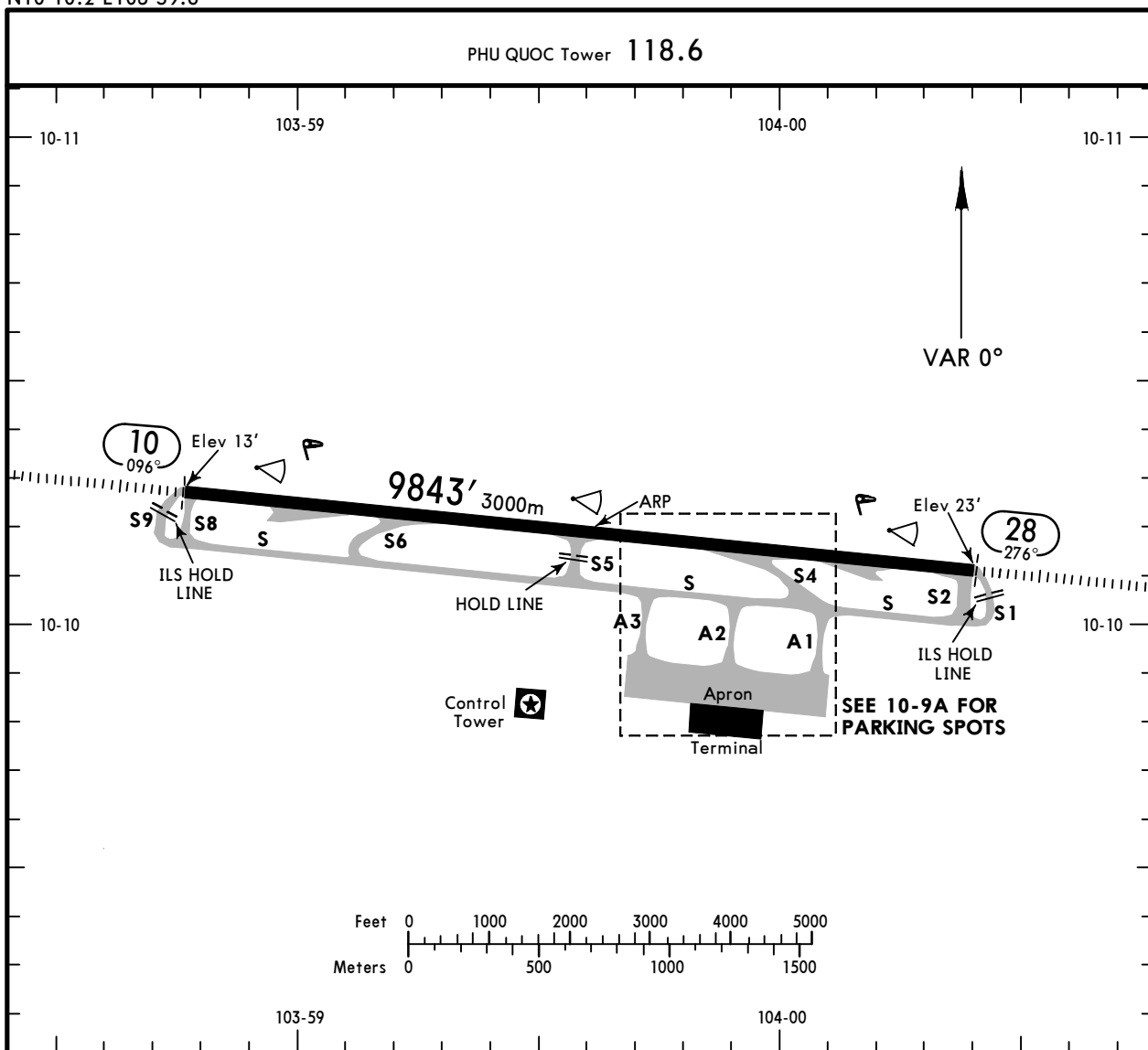
JEPPESEN

PHU QUOC, VIETNAM

Apt Elev 23'
N10 10.2 E103 59.6

28 DEC 18 (10-9)

PHU QUOC INTL



ADDITIONAL RUNWAY INFORMATION

RWY	HIRL	CL	HIALS	PAPI-L	RVR	USABLE LENGTHS		TAKE-OFF	WIDTH
						Threshold	Glide Slope		
10							8860' 2701m		148'
28				PAPI-R	RVR		8779' 2676m		45m

1 2 3 TAKE-OFF

	Take-Off Alternate Airport Filed		Take-Off Alternate Airport not Filed
	Rwy 28	Rwy 10	
A			Available Landing Minimums
B			
C	RVR 800m	RVR 1000m	
D			

- 1** Take-off alternate airports: For international flights - Tan Son Nhat Intl.
For domestic flights - Tan Son Nhat Intl, Rach Gia, Can Tho.
- 2** At Rach Gia airport, the runways, taxiways and aprons are capable of handling ATR72, King Air B200 and equivalent.
- 3** When the aircraft cannot choose any take-off alternate airports as mentioned above, lowest weather minima for take-off will equal landing minima for an available instrument approach procedure.

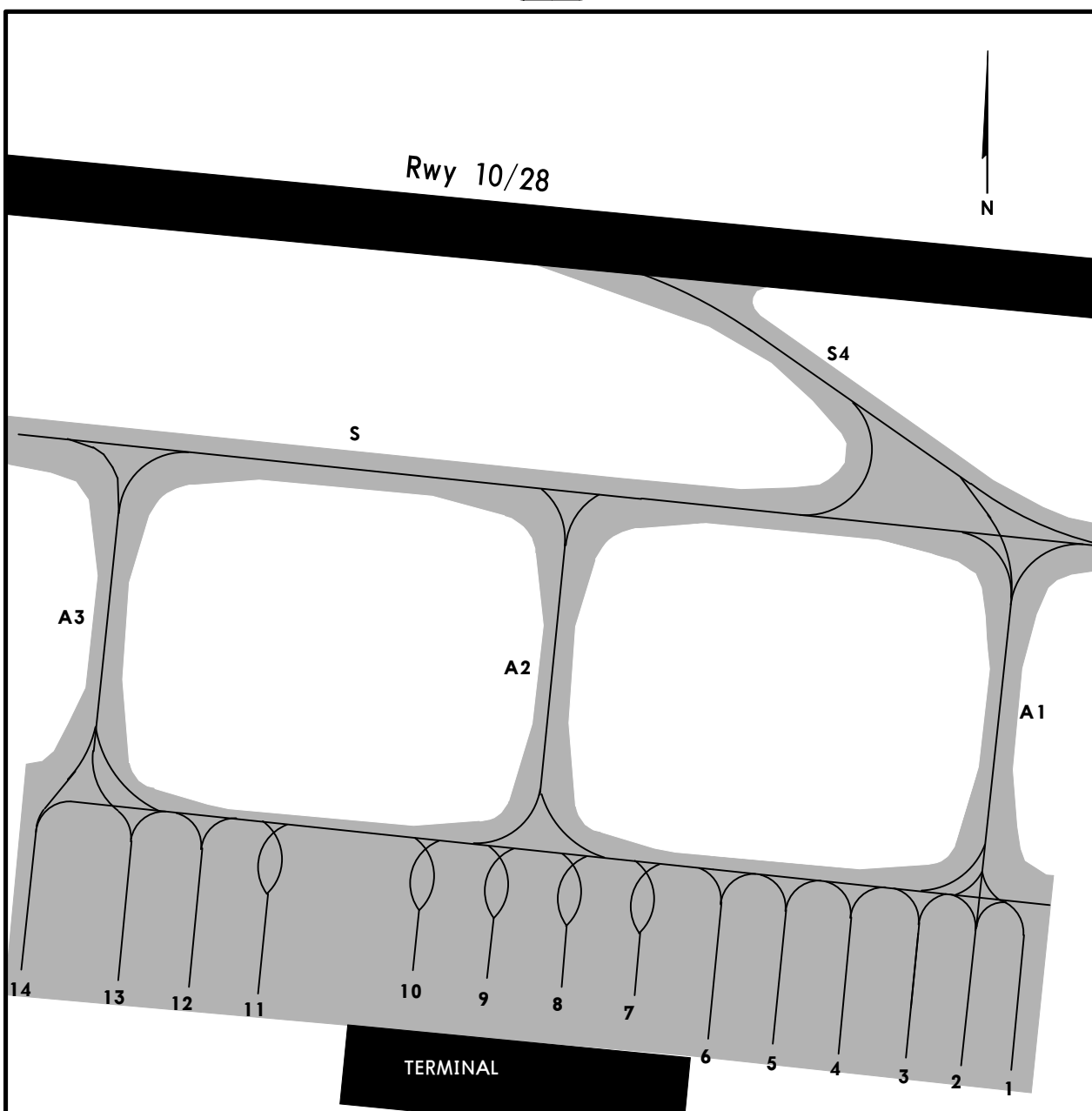
VVPQ/PQC

JEPPESEN

PHU QUOC, VIETNAM

28 DEC 18 (10-9A)

PHU QUOC INTL



PARKING SPOT COORDINATES

SPOT NO.	COORDINATES
1 thru 3	N10 09.8 E104 00.1
4 thru 6	N10 09.8 E104 00.0
7 thru 9	N10 09.8 E103 59.9
10, 11	N10 09.9 E103 59.8
12 thru 14	N10 09.9 E103 59.7

REMARKS

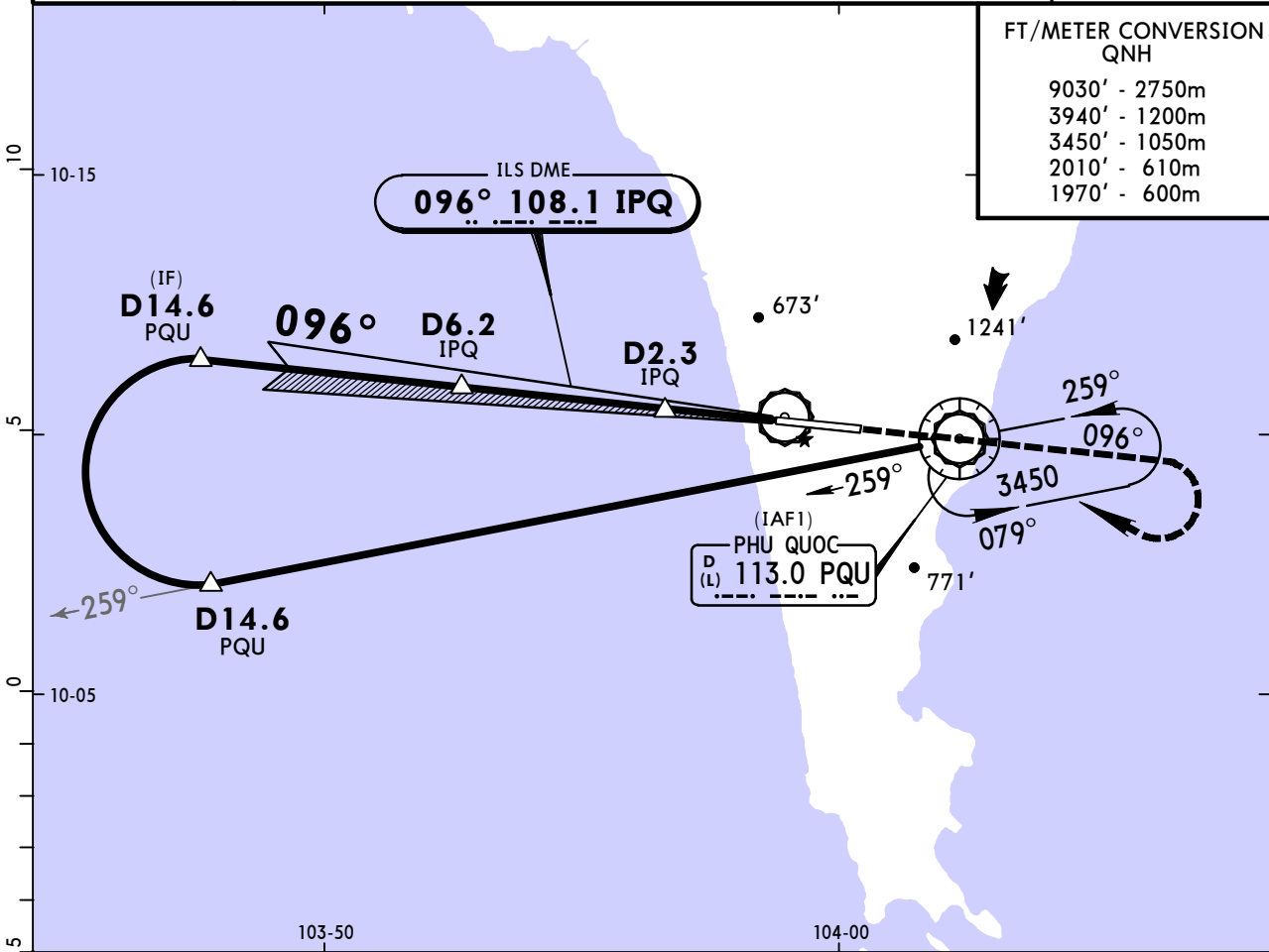
- Aircraft taxi stand for parking following by signal man.
- Aircraft are pushed back and towed out for departure.
- Stands number 1, 2: used for aircraft up to ATR72 and equivalent.
- Stands number 3, 4, 5, 6, 12, 13 and 14: used for aircraft up to A321 and equivalent.
- Stands number 7, 8, 9, 10 and 11: used for aircraft up to B747-400 and equivalent.

VVPQ/PQC
PHU QUOC INTL



PHU QUOC, VIETNAM
ILS Rwy 10

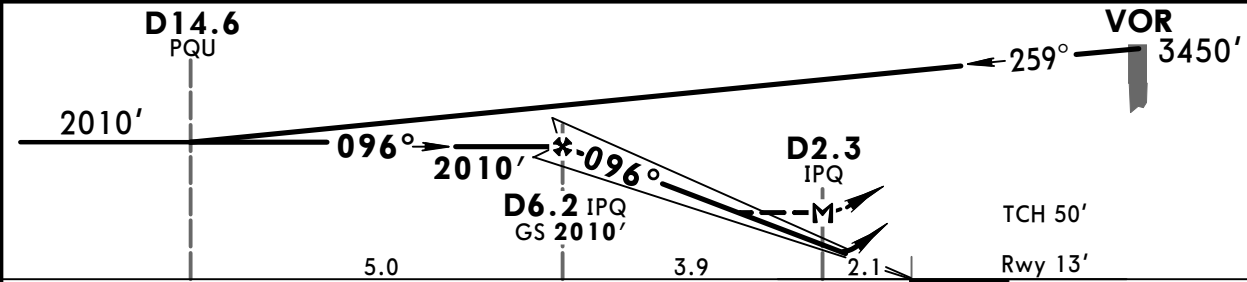
PHU QUOC Tower 118.6					
LOC IPQ 108.1	Final Apch Crs 096°	GS D6.2 IPQ 2010' (1997')	ILS DA(H) Refer to Minimums	Apt Elev 23' Rwy 13'	
MISSED APCH: Maintain final approach track climbing to 1970', turn RIGHT to PQU VOR at 3450' to join holding pattern or follow ATC instructions.					
Alt Set: hPa Rwy Elev: 0 hPa		Trans level: FL 100		Trans alt: 9030'	
1. VOR, DME required.					MSA PQU VOR



FT/METER CONVERSION
QNH

9030'	2750m
3940'	1200m
3450'	1050m
2010'	610m
1970'	600m

IPQ DME	6.2	5.0	4.0	3.0	2.3
ALTITUDE	2010'	1620'	1300'	980'	750'



Gnd speed-Kts	70	90	100	120	140	160	HIALS PAPI 1970' 3450' PQU 113.0	
GS	3.00°	372	478	531	637	743		849
MAP at D2.3 IPQ or FAF to MAP	3.9	3:21	2:36	2:20	1:57	1:40		1:28

STRAIGHT-IN LANDING RWY 10		CEILING REQUIRED		CIRCLE-TO-LAND	
ILS		LOC (GS out)			
DA(H)	A: 656' (643') C: 673' (660')	DA(H)	B: 669' (656') D: 689' (676')	MDA(H)	750' (737')
	FULL	ALS out		ALS out	

PANS OPS	A	660' - 1400m	760' - 2000m	A
	B			B
	C	670' - 1400m	760' - 3600m	C
	D	690' - 1400m	760' - 4000m	D

NOT AUTHORIZED

VVPQ/PQC
PHU QUOC INTL

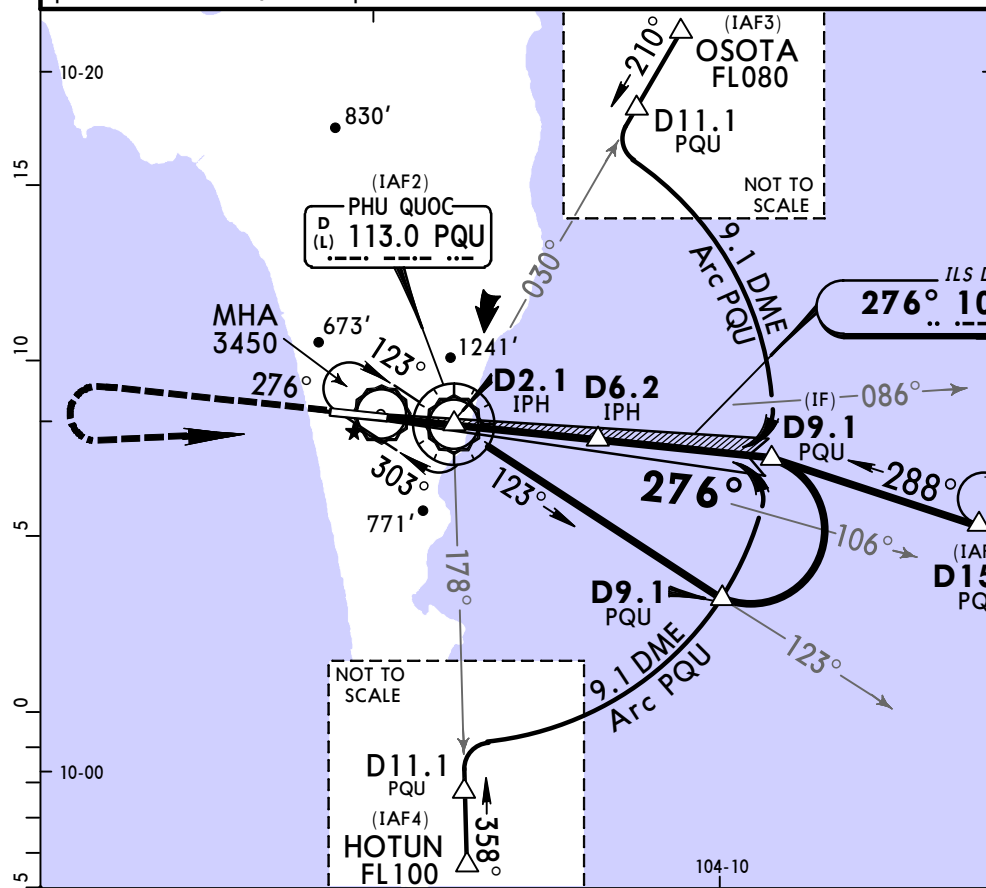
JEPPESSEN
29 JUL 16 (11-2)

PHU QUOC, VIETNAM
ILS Rwy 28

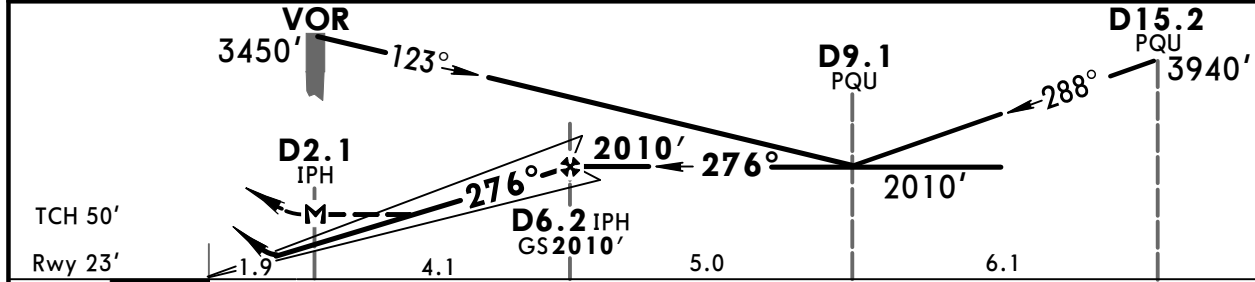
PHU QUOC Tower 118.6					
LOC IPH 108.7	Final Apch Crs 276°	GS D6.2 IPH 2010'(1987')	ILS DA(H) Refer to Minimums	Apt Elev 23' Rwy 23'	<p>MSA PQU VOR</p>
<p>MISSED APCH: Maintain final approach track climb to 3450', passing 1970' turn LEFT to PQU VOR to join holding pattern or follow ATC instructions.</p>					
<p>Alt Set: hPa Rwy Elev: 1 hPa Trans level: FL 100 Trans alt: 9030'</p> <p>1. VOR, DME required. 2. Minimum vertical separation of 1000' is applied between holding patterns at PQU VOR and PQU VOR R-101/D15.2. 3. Approach procedure from HOTUN IAF via PQU VOR 9.1 DME Arc is only used when no firing activities are taking place within Phu Quoc airspace.</p>					

FT/METER CONVERSION
QNH

9030'	2750m
3940'	1200m
3450'	1050m
2010'	610m
1970'	600m



IPH DME	2.1	3.0	4.0	5.0	6.2
ALTITUDE	700'	980'	1300'	1620'	2010'



Gnd speed-Kts	70	90	100	120	140	160	HIALS PAPI 1970' ↑ 3450' ← PQU 113.0
GS	372	478	531	637	743	849	
MAP at D2.1 IPH or FAF to MAP	4.1	3:31	2:44	2:28	2:03	1:45	

STRAIGHT-IN LANDING RWY 28		CEILING REQUIRED	CIRCLE-TO-LAND
ILS DA(H) A: 367' (344') C: 387' (364') B: 377' (354') D: 400' (377')	LOC (GS out) MDA(H) 700' (677')		
FULL	ALS out	ALS out	

PANS OPS	A	370' - 1100m	370' - 1200m	680' - 1600m	A
	B				B
	C	380' - 1100m	380' - 1200m	680' - 2800m	C
	D			680' - 3200m	D

VVPQ/PQC

JEPPesen

PHU QUOC, VIETNAM

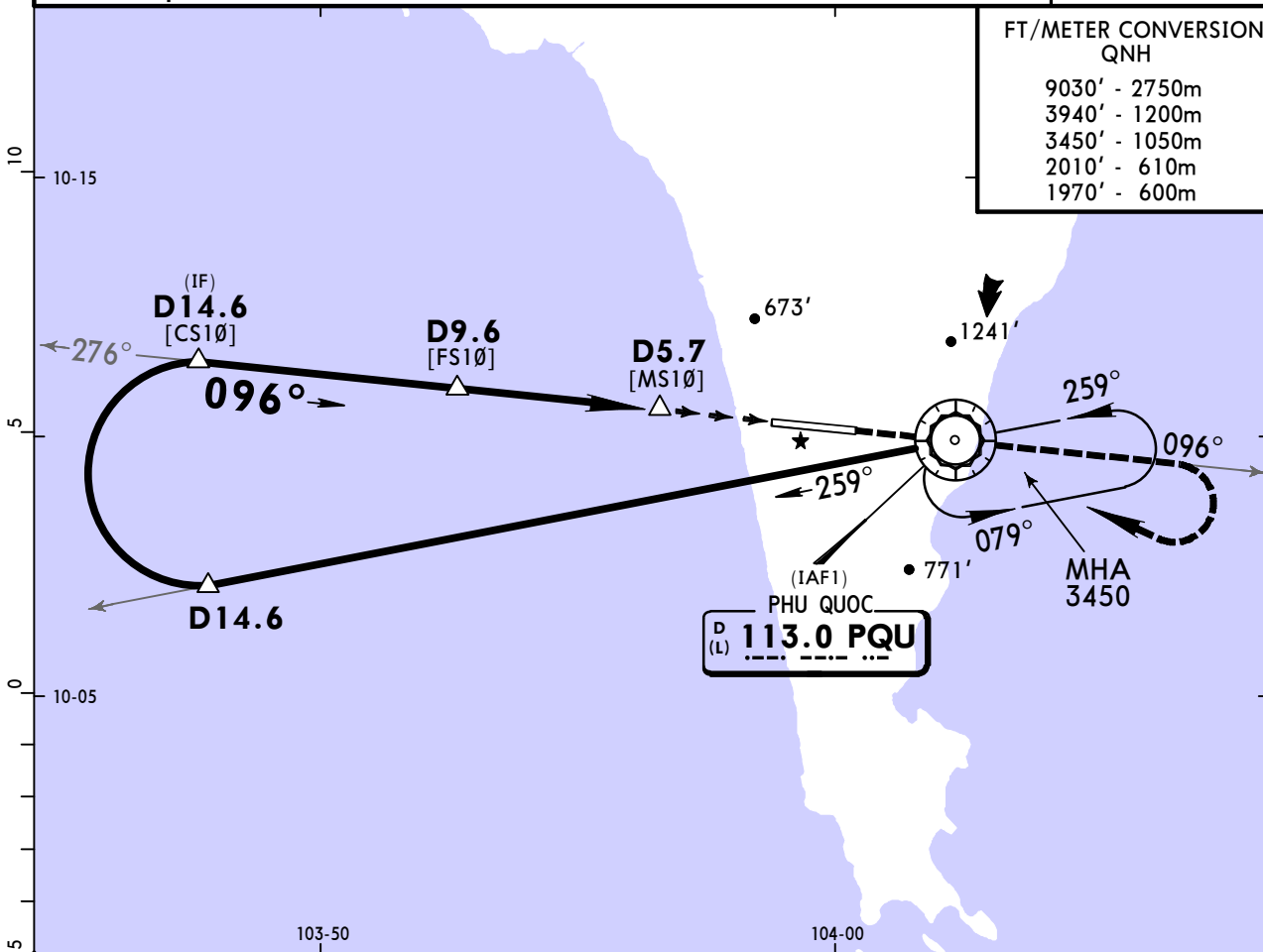
PHU QUOC INTL

29 JUL 16 (13-1)

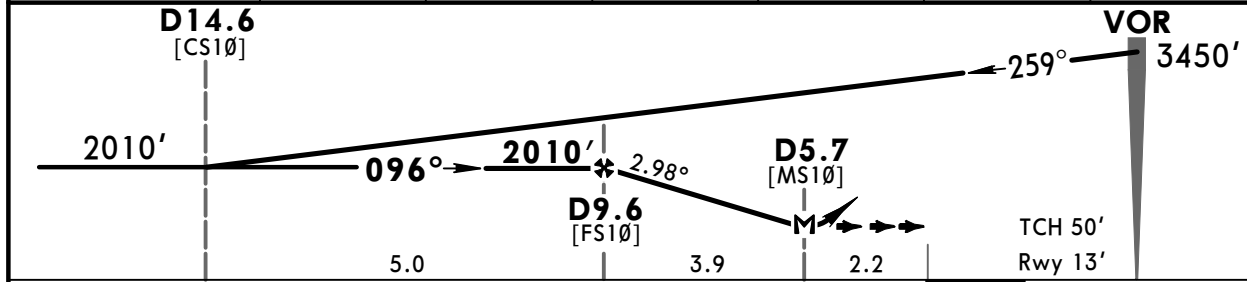
VOR Rwy 10

PHU QUOC Tower 118.6					
VOR PQU 113.0	Final Apch Crs 096°	Minimum Alt D9.6 2010' (1997')	MDA(H) 750' (737')	Apt Elev 23' Rwy 13'	
MISSED APCH: Maintain final approach track climbing to 1970', turn RIGHT to PQU VOR at 3450' to join holding pattern or follow ATC instructions.					
Alt Set: hPa		Rwy Elev: 0 hPa	Trans level: FL 100	Trans alt: 9030'	MSA PQU VOR

FT/METER CONVERSION QNH	
9030'	2750m
3940'	1200m
3450'	1050m
2010'	610m
1970'	600m



PQU DME	9.6	9.0	8.0	7.0	6.0	5.7
ALTITUDE	2010'	1800'	1480'	1160'	840'	750'



Gnd speed-Kts	70	90	100	120	140	160		1970'	3450'	PQU	
Descent Angle	2.98°	369	474	527	633	738		843	↑	RT	113.0
MAP at D5.7 or FAF to MAP	3.9	3:21	2:36	2:20	1:57	1:40		1:28			

STRAIGHT-IN LANDING RWY 10		CEILING REQUIRED	CIRCLE-TO-LAND
MDA(H) 750' (737')		ALS out	
CEILING-VISIBILITY			

PANS OPS	A	760' - 2000m	A	NOT AUTHORIZED
	B		B	
	C	760' - 3600m	C	
	D	760' - 4000m	D	

VVPQ/PQC

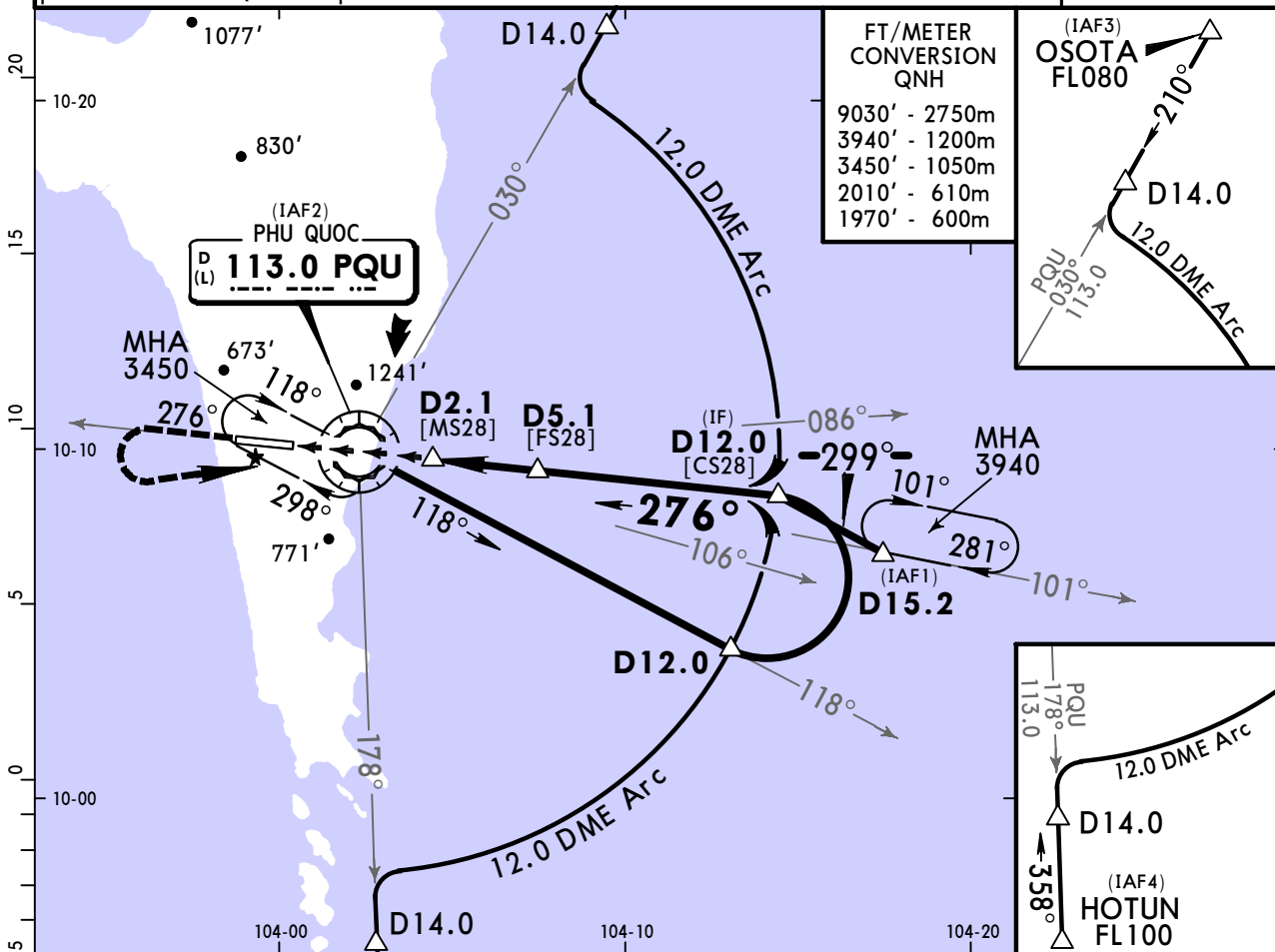
PHU QUOC INTL

JEPPesen
29 JUL 16 (13-2)

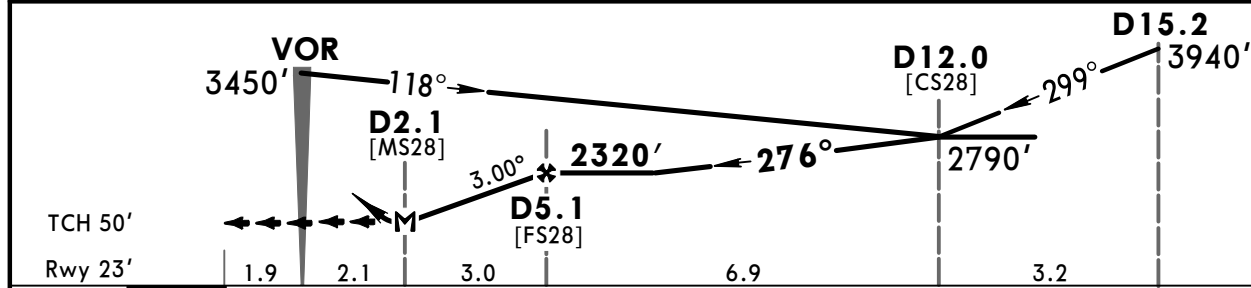
PHU QUOC, VIETNAM

VOR Rwy 28

PHU QUOC Tower 118.6				
VOR PQU 113.0	Final Apch Crs 276°	Minimum Alt D5.1 2320' (2297')	MDA(H) 1350' (1327')	Apt Elev 23' Rwy 23'
MISSED APCH: Maintain final approach track climb to 3450', passing 1970' turn LEFT to PQU VOR to join holding pattern or follow ATC instructions.				
Alt Set: hPa Rwy Elev: 1 hPa Trans level: FL 100 Trans alt: 9030' 1. DME required. 2. Minimum vertical separation of 1000' is applied between holding patterns at PQU VOR and PQU VOR R-101/D15.2. 3. Approach procedure from HOTUN IAF via PQU VOR 12.0 DME Arc is only used when no firing activities are taking place within Phu Quoc airspace.				



PQU DME	2.1	3.0	4.0	5.1
ALTITUDE	1350'	1650'	1970'	2320'



Gnd speed-Kts	70	90	100	120	140	160	HIALS PAPI 1970' 3450' PQU ↑ LT 113.0
Descent Angle 3.00°	372	478	531	637	743	849	
MAP at D2.1 or FAF to MAP	3.0	2:34	2:00	1:48	1:30	1:17	1:08

STRAIGHT-IN LANDING RWY 28		CEILING REQUIRED	CIRCLE-TO-LAND
MDA(H) 1350' (1327')			
ALS out			

PANS OPS	A	CEILING-VISIBILITY	ALS out
	B	1330' - 5000m	
	C		
	D		NOT AUTHORIZED

CHANGES: IAF labels.

Chart changes since cycle 01-2019

ADD = added chart, REV = revised chart, DEL = deleted chart.

ACT	PROCEDURE IDENT	INDEX	REV DATE	EFF DATE
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PHU QUOC, (PHU QUOC INTL - VVPQ)

TERMINAL CHART CHANGE NOTICES

No Chart Change Notices for Airport VVPQ